

IV. Zoning Regulations:

6. Parking, Loading, & Access

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Section 6 - Parking, Loading, & Access: General Requirements

A. General Requirements.

1. Intent. The following provisions are established to accomplish the following:
 - a. Ensure an appropriate level of vehicle parking, loading, and storage to support a variety of land Uses.
 - b. Provide appropriate site design standards to mitigate the impacts of parking lots on adjacent land Uses and Zoning Districts.
 - c. Provide specifications for vehicular site access.
2. Applicability. This Section shall apply to all new development and changes in Use or intensity of Use for existing development, in all Zoning Districts.
3. General Requirements. The scope of the regulations in this Section are as follows:
 - a. Compliance. Compliance with the standards outlined shall be attained in the following circumstances:
 - (1) Development of all new parking facilities, loading facilities, and driveways.
 - (2) Improvements to existing parking facilities, loading facilities, and driveways, including resurfacing, striping, fencing, curbs, walkways, or landscape installation.
 - (3) Changes in intensity through the addition of Dwelling Units, Gross Floor Area, seating capacity, or other units of measurement specified herein for required parking and loading facilities.
 - (4) Change in Use.
 - b. Damage or Destruction. When a Use that has been damaged or destroyed by fire, collapse, explosion, or other cause is reestablished, any associated off-street parking spaces or loading facilities must be reestablished in an amount equal to the number maintained at the time of damage or destruction or if this quantity is in excess of the standards outlined herein, the minimum standards must be met.
 - c. Access. All off-street parking and loading facilities shall open directly onto an aisle, alley, or driveway designed to provide safe access to such facilities.
 - d. General Use. All off-street parking and loading facilities shall be used only for the temporary storage of motor vehicles related to the premises. The storage of merchandise or the repair of vehicles shall be prohibited in such areas, unless otherwise specified in this Ordinance.
 - e. Materials. All parking and loading facilities and driveways shall be constructed using asphalt,

- concrete, pavers, or other Semi-Pervious material approved by the Code Administrator.
- f. Location. Each Building Type (IV.3 Building Type Standards) includes the permitted yard locations of parking facilities (surface or structured), loading facilities, and points of access.
- g. Maintenance of Off-Street Parking. All off-street parking spaces required by this Ordinance shall be continuously maintained by the property Owner.
- h. Site Plan Approval Required. Refer to I.4.H-I Site Plan Review for more information.
- i. Certificate of Occupancy. Refer to I.4.Q Certificate of Occupancy for more information.

B. Parking Requirements.

1. General Requirements. Off-street parking spaces shall be provided in conformance with Tables 6.B-1 Required Vehicular Parking and 6.B-2 Bicycle Parking.
 - a. Required Accessible Parking. Parking facilities accessible for persons with disabilities shall be in compliance with or better than the standards detailed in Tennessee Code Annotated (T.C.A.) §55-21-105, including quantity, size, location, and accessibility.
 - (1) Refer to Table 6.B-3 Required Accessible Parking Spaces.
 - (2) Each accessible space provided shall be included in the overall number of required spaces for a Use.
 - (3) One (1) of every eight (8) accessible spaces provided, with a minimum of one (1) per Lot, shall be designed as van accessible.
 - b. Use of Off-Street Parking Areas.
 - (1) Availability of Parking Spaces. Required off-street parking spaces shall be available to residents, customers, employees, or guests of the Principal Use(s).
 - a. Spaces that are owned by or dedicated for use by other persons or for display vehicles shall not be counted toward the off-street parking requirements unless a shared parking agreement is in place.
 - (2) Storage Prohibited in Parking Areas. Required off-street parking spaces shall be dedicated for the parking of vehicles in operating condition and shall not be used for long-term storage or display of vehicles, materials, or goods, unless otherwise stated in this Ordinance.
 - (3) Vehicle Repair and Service in Districts NC1, C1, C2, NO, O, and R1-9. No motor vehicle repair work or service of any kind shall be permitted in association with parking or loading

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	Use	Vehicular Spaces Required
Infrastructure	Parking Lot	-
	Utility & Infrastructure	1/Employee at lrgst shift + 1/Company vehicle
	Wireless Transmission Facility	1/Employee at lrgst on site shift + 1/Company vehicle
	Neighborhood Open Space/Civic Open Space	-
	General Open Space	2/Acre at Code Administrator's authority; 4/Acre at Code Administrator's authority when ball fields and courts included
	Large Scale Outdoor Recreation	
	Country Club	1/250 sq.ft. + 1/Employee at largest shift + as applicable; 40/9 holes of golf + .25/facility occupancy for tennis and swim
	Golf Course	60/9 holes
	Membership Sports Clubs	1/Employee at lrgst shift + .25/Facility occupancy
	Riding Academy & Stables	1/Employee at lrgst shift + 1/Stall
Open Space & Recreation	Sports and Recreation Camps	3/1000 sq.ft. building + 3/1000 sq.ft. of land
	Swim Club	1/Employee at lrgst shift + .25/Facility occupancy
	Tennis Club	1/Employee at lrgst shift + .25/Facility occupancy
	Preserve/Conservation	Per Code Administrator
	Shooting/Archery Range	1.50/Target
	Agriculture Equipment & Supply	1/250 sq.ft.
	Farming	-
	Animal Boarding	1.35/Employee
	Natural Resource Harvesting & Management	1/Employee at lrgst on site shift + 1/Company vehicle
Agriculture		

	Use	Vehicular Spaces Required
Residential	Single Family/Single Family Attached	1/Unit
	Multiple Family: Studio/Efficiency	1.25/Unit
	Multiple Family: 1 Bdrm	1.5/Unit
	Multiple Family: 2 Bdrm	1.75/Unit
	Multiple Family: 3+ Bdrm	2/Unit
	Accessory Family Unit	1/Unit
	Bed and Breakfast	1.25/Room
	Hospice	.25/Bed & 1/Employee
	Hotel/Motel	1.5/Room, Secondary Uses at 65%
	Ind. & Assisted Living/Nursing Home	.25/Bed & 1/Employee
Civic/Institutional	Neighborhood, General, Large Scale Assembly	.25/Seat or Bldg capacity
	Hospital	1/4 Beds, 1/Employee
	Library/Museum	1/300 sq.ft.
	Police & Fire	.75/Employee + 1/Office vehicle
	Post Office	1/300 sq.ft.
	School : Pre K to Jr. High	1.25/Employee
	School : High School	1/Employee + .5/Students in 11th and 12th grades
	School : Higher Education	.75/Employee + .5/Student
	Neighborhood, General, Large Scale Retail	1/250 sq.ft.
	Outdoor Sales Lot	1/250 sq.ft. of sales area
Service	Neighborhood Personal, General, Regional Services	1/250 sq.ft.
	Day Care	1.35/Employee
	Outdoor Recreation	1/500 sq.ft. + 1/4000 sq ft of land
	Vehicle Service	1/300 sq.ft.
Office	Neighborhood, General Office	1/250 sq ft.
	Research and Development	1/250 sq ft. of sales area
Production	Craftsman	1/Employee at lrgst shift + 1/250 sq.ft. of retail space
	Production & Assemblage	1/Employee at lrgst shift + 1/Company vehicle
	Warehouse, Packing, & Distribution	1/Employee at lrgst shift + 1/Company vehicle
	Manufacturing	1/Employee at lrgst shift + 1/Company vehicle
	Sexually Oriented Business	1/250 sq.ft.

Table 6.B-1. Required Vehicular Parking.

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- facilities provided in these districts, with the exception of emergency service required to start vehicles.
- c. Requirements for Unlisted Uses. Upon receiving a site plan, occupancy permit, or other permit application for a Use not specifically addressed in this Section, the Code Administrator is authorized to apply off-street parking standards specified for the Use deemed most similar to the proposed Use. In instances where an equivalent may not be clearly determined, the Code Administrator may require the Applicant to submit a parking study or other evidence that will help determine the appropriate requirements.
 - d. Private Off-Premises Parking. Parking shall be provided on-site whenever possible. Where private off-site parking facilities are approved, such facilities shall be in the same possession as the zoning Lot occupied by the building or Use to which the parking facilities are accessory.
 - (1) Such possession may be either by deed or lease, guaranteeing availability of the parking commensurate with the Use served by the parking.
 - (2) The agreement providing for the Use of off-site parking, executed by the parties involved, shall be in a form approved by and filed with the Code Administrator.
 - (3) The deed or lease shall require the Owner to maintain the required number of parking facilities for the duration of the Use served or of the deed or lease, whichever shall terminate sooner.
2. Required Vehicular and Bicycle Parking. Tables 6.B-1 and 6.B-2 outline the required vehicular and bicycle parking requirements.
 - a. Organized by Use. The parking requirements are organized by Use, in a similar fashion to Table 2.L-1 Use Table in IV.2 Uses.
 - (1) Parking rates are provided for general Use categories; these numbers are applicable for all of the Uses within these categories.
 - (2) If a specific Use requires a different parking rate than its Use category, it is also listed in Tables 6.B-1 and 6.B-2 Required Vehicular and Bicycle Parking.
 - b. Vehicular Spaces Required. The vehicular spaces required column indicates the required off-street parking ratio, which may be subject to credits and other reductions and a maximum number, as are detailed in this Section.
 - (1) Three or Fewer Spaces. When the Application of the specified vehicular off-street parking provisions results in a requirement of not more than three (3) spaces on a single zoning Lot in NC1 and NO districts, such parking spaces need not be provided for non residential Uses.
 - (2) Subdivision Type III. Code Administrator can exempt non residential Uses in NC1 and NO districts developed in Subdivision Type III from the parking space requirements when sufficient off-site and on-street parking are present/ within six hundred and sixty (660) feet from the Lot measured along dedicated pedestrian pathways.
 - c. Required Bicycle Parking. The Required Bicycle Parking column indicates the minimum bicycle parking ratio for a given Use.

Use	Bicycle Spaces Required
Multitple Family (all sizes)	1/2 Vehicular Spaces for buildings with 8+ units
Civic/Institutional	1/10 Vehicular Spaces, min of 4
Retail	1/10 Vehicular Spaces
Service	1/10 Vehicular Spaces
Office	1/10 Vehicular Spaces
Neighborhood Open Space/Civic Open Space	Per Code Administrator
General Open Space	Per Code Administrator
Large Scale Open Space	1/10 Vehicular Spaces
Preserve/Conserve	Per Code Administrator

Table 6.B-2. Required Bicycle Parking.

Total Spaces	Accessible Spaces
1 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 to 300	7
301 to 400	8
401 to 500	9
501 to 1000	2% of total
1001 +	20 + 1 per each 100 over 1000

Table 6.B-3. Required Accessible Parking Spaces.

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- (1) Bicycle parking is not required for Uses not listed.
 - (2) Bicycle parking is not required for Uses less than 2,500 square feet in size.
 - (3) No Use, other than Civic/Institutional or Open Space and Recreation (see IV.2.D and IV.2.J), is required to accommodate more than twenty (20) bicycles.
3. Computation. Off-street parking spaces shall be calculated using the following information.
- a. Area Measurements. The following units of measurements shall be utilized to calculate parking requirements.
 - (1) Dwelling Unit. Parking standards for residential buildings shall be computed using Dwelling Unit as the unit of measure.
 - (2) Gross Square Footage. Unless otherwise expressly stated, parking standards for non residential Uses shall be computed on the basis of Gross Floor Area in square feet.
 - (3) Occupancy- or Capacity-Based Measurements. Parking spaces required per available seat or per employee, student, or occupant shall be based on the greatest number of persons on the largest shift, the maximum number of students enrolled, or the maximum fire-rated capacity, whichever measurement is applicable.
 - b. Fractions. When computation of the number of required off-street parking spaces results in a fractional number, any result of 0.5 or more shall be rounded up to the next consecutive whole number. Any fractional result of less than 0.5 may be rounded down to the previous consecutive whole number.
 - c. Multiple Uses on a Lot. When there are multiple Uses on a Lot, required spaces shall be calculated as an amount equal to the total requirements for all Uses on the Lot, unless the Uses qualify for shared, cooperative, or other credits to reduce parking.
 - d. Maximum Allowable Vehicular Spaces. No Use shall provide greater than ten (10) percent over the minimum parking requirement without incorporating at least two (2) of the following mitigating design features. In no case shall a Use provide over twenty-five (25) percent more parking spaces than the requirement.
 - (1) The surface of all excess parking spaces shall be a Semi-Pervious Surface.
 - (2) The frontage buffer shall be increased in width by thirty (30) percent and the heavy side and rear buffer shall be used, regardless of the adjacent Uses (refer to IV.5.J-K for more information).
4. Shared Vehicular Parking. An arrangement in which two (2) or more non residential Uses with different peak parking demands use the same off-street parking spaces to meet their off-street parking requirements.
- a. General Provisions. Through review of the site plan (refer to I.4.H-I) the Code Administrator may permit up to one hundred (100) percent of the parking required for a daytime Use to be supplied by the off-street parking spaces provided for a nighttime or Sunday Use and vice versa.
 - b. Approval. In order to approve a shared parking arrangement, the Code Administrator must find, based on competent evidence provided by the Applicant, that there is no substantial conflict in the principal operating hours of the Uses for which the sharing of parking is proposed.
 - c. Description of Uses with Weekday, Nighttime, and Sunday Peak Parking.
 - (1) The following Uses are considered predominantly weekday Uses: office and industrial Uses and other similar Uses as authorized by the Code Administrator.
 - (2) The following Uses are typically considered predominantly nighttime or Sunday Uses: entertainment Uses, eating and drinking establishments, Assembly Uses, auditoriums accessory to schools and other similar Uses with peak activity at night or on Sundays, as authorized by the Code Administrator.
5. Cooperative Vehicular Parking. An arrangement in which two (2) or more categories of non residential Uses (Refer to Table 2.L-1 in IV.2 Uses; Use categories include civic/institutional, retail, service, office, etc.) provide their required off-street parking as follows:
- a. General Provisions. Cooperative parking will be approved in accordance with the following:
 - (1) A twenty-five (25) percent reduction is permitted when four (4) or more Use categories are involved.
 - (2) A fifteen (15) percent reduction is permitted when three (3) Use categories are involved.
 - (3) A ten (10) percent reduction is permitted when two (2) Use categories are involved.
 - b. Uses in Different Buildings. Through review of the site plan (refer to I.4.H-I) the Code Administrator

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may approve the cooperative agreement if any of the Uses are not located in the same structure or building.

- c. Location of Cooperative Parking. Any cooperative parking arrangements must be within six hundred sixty (660) feet, measured from the entrance of the Use to the closest parking space within the cooperative parking lot along measured along a dedicated pedestrian path.
- d. Off-Site Cooperative Parking Agreement. An agreement providing for cooperative use of off-site parking spaces, executed by the parties involved, shall be reviewed by the Code Administrator during through review of the site plan (refer to I.4.H-I).
 - (1) Off-site cooperative parking arrangements shall continue in effect only as long as the agreement remains in force.
 - (2) If the agreement is no longer in force, then parking must be provided as otherwise required in this Section.

- 6. Parking Credits. In NC1, C1, C2, NO, and O Districts, the vehicular parking standards in Table 6.B-1 for non residential Uses may be reduced by achieving parking in one (1) or all of the following credits.
 - a. On-Street Parking Credit. Designated on-street parking within three hundred sixty (660) feet of any Lot Line may be credited against the parking requirement at a rate of one (1) credit for every two (2) on-street parking spaces within six hundred sixty (660) feet.
 - b. Public Parking Credit. Public parking within six hundred sixty (660) feet of any Lot Line may be credited against the parking requirement at a rate of one (1) credit for every three (3) public parking spaces within six hundred sixty (660) feet.

C. Parking Design Standards.

- 1. Vehicular Off-Street Parking Lots. The design or redesign of all off-street parking facilities shall be subject to the site plan approval procedure described in I.4.H-I.
 - a. Vehicular Parking Space Dimensions. The appropriate dimensions for parking spaces are outlined in Table 6.C-1 Parking Space Dimensions and Figure 6.C-1 Parking Lot Layout. The width of a parking space shall be measured from the center of a stripe.
 - b. Wheel Stops. Install wheel stops or bumper guards when parking is adjacent to a pedestrian pathway or landscaped area. Such stops or guards shall be properly anchored or secured.
 - c. Location of Parking. Refer to IV.3 Building Type Standards for information on the location of parking facilities.
 - d. Landscape Screening. All parking areas shall meet the requirements of in IV.5 Landscape Standards.
 - e. Pavement Materials. Open air parking lots shall be constructed with any of the following materials.
 - (1) Paving materials with a Solar Reflectance Index (SRI) of at least 29.
 - (2) Pervious pavement.

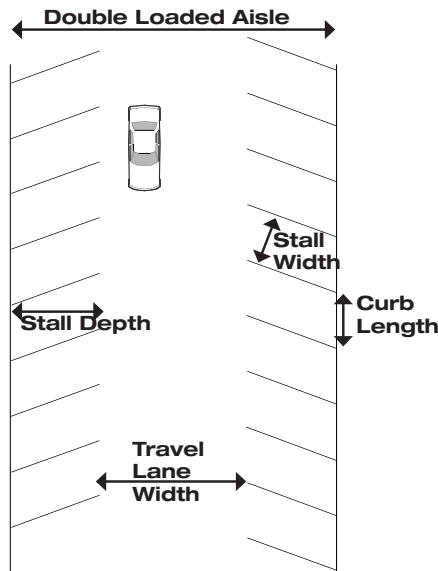


Figure 6.C-1. Parking Lot Layout.

Angle (degrees)	Curb Length (feet)	Stall Width (feet)	Stall Depth (feet)	Travel Lane Width: One-Way (feet)	Travel Lane Width: Two-Way (feet)
0	20	7	-	12	20
45	12	8.5	17	12	20
60	10	8.5	18	18	20
90	9	8.5	18	22	24

Table 6.C-1. Parking Space Dimensions.

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- f. Signage. Accessory signs shall be permitted on parking areas in accordance with the provisions specified in the appropriate sign regulations.
 - (1) City may require the posting of traffic control signs.
 - (2) All traffic control signs shall be installed in accordance with the Manual for Uniform Traffic Control Devices (MUTCD) and its most recent amendments.
- 2. Pedestrian Access. All parking lots with more than two (2) double-loaded aisles shall provide internal pedestrian pathway(s) within the parking area.
 - a. Dimension. The pathway shall be a minimum of six (6) feet in width.
 - b. Quantity. One (1) pathway is required for every two (2) double loaded aisles.
 - c. Location. The pathway shall be centrally located within the parking area to serve a maximum number of parking stalls.
 - (1) Pathways shall provide direct connections to the Principal Structure(s) entrances.
 - (2) At least one (1) pathway shall provide a direct connection between adjacent Vehicular Rights-of-Way and/or trails and the Principal Structure's entrance.
 - d. Pathway Delineation. Pedestrian pathways should be clearly marked with striping or through the use of alternative materials, such as pavers.
 - e. Amenities. Amenities such as lighting and bollards are encouraged along pedestrian pathways and building entrances.
- 3. Bicycle Parking. Bicycle parking (refer to Table 6.B-2 Required Bicycle Parking for quantity required) shall be designed and located as follows.
 - a. Dimensions. Required bicycle parking spaces shall have minimum dimensions of two (2) feet in width and six (6) feet in length.
 - b. Location. Bicycle parking may be located in the following locations:
 - (1) Indoor or outdoor spaces are permitted, provided they are located on the Lot with which they are associated.
 - (2) Spaces located within individual Dwelling Units may not be counted toward bicycle parking requirements.
 - c. Signage. If required bicycle parking for public Use is not visible from the street, signs must be posted indicating their location.
 - d. Maintenance and Lighting. Areas used for required bicycle parking must be well-lit with acceptable drainage to be reasonably free of mud and standing water.

D. Loading Requirements.

- 1. General Requirements. All loading facilities shall adhere to the following requirements.
 - a. Use of Off-Street Loading Areas. Space allocated to any off-street loading Use shall not be used to satisfy the space requirements for any off-street parking facilities or portions thereof.
 - b. Location. Unless otherwise specified, all required loading facilities shall be located on the same Lot as the Use to be served. No loading space shall block or project into a street, alley, access drive, or parking area.
 - c. Building Frontage. Loading facilities shall be located on the rear of side building facades only.
 - d. Access. Loading facilities shall have clear access onto an alley or be connected to an alley or street via a driveway.
 - (1) Direct access to a public way, other than an alley, is prohibited.
 - (2) Each required off-street loading space shall be designed with appropriate means of vehicular access to a street or alley in a manner which will least interfere with traffic movement.
- 2. Loading Requirements. Non residential Uses shall provide off-street loading spaces in compliance with Table 6.D-1 Required Loading Facilities.
- 3. Computation. Loading facilities shall be calculated using the following information.
 - a. Gross Square Footage. Unless otherwise expressly stated, loading standards for non residential buildings shall be computed on the basis of Gross Floor Area in square feet.
 - b. Fractions. When computation of the number of required off-street loading spaces results in a fractional number, any result of 0.5 or more shall be rounded up to the next consecutive whole number. Any fractional result of less than 0.5 may

Gross Floor Area (sq. ft)	Loading Spaces Required
Under 5,000	0
5,000 to 20,000	1
20,001 to 100,000	1 + 1 for each 20,000 over 20,001
100,001 +	5 + 1 for each 50,000 over 100,001

Table 6.D-1. Required Loading Facilities.

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be rounded down to the previous consecutive whole number.

- c. Shared or Central Loading Facilities. Shared or central loading facilities are permitted if the following conditions are met.
 - (1) Each zoning Lot served shall have direct access to the central loading area without crossing streets or alleys.
 - (2) Total off-street loading spaces provided shall meet the minimum requirements herein specified, based on the sum of the several types of Uses served unless reviewed and approved by the Code Administrator through site plan review (refer to I.4.H-I).
 - (3) No zoning Lot served shall be more than five hundred (500) feet from the central loading area.

E. Loading Facilities Design Standards.

1. General Requirements. All off-street loading facilities shall comply with the following design standards. These plans are subject to approval through the Site Plan Review Process in I.4.H-I.
2. Dimensions. A standard off-street loading space shall be a minimum of ten (10) feet in width by twenty-six (26) feet in length and an oversized loading space shall be a minimum of twelve (12) in width and forty (40) feet in length, exclusive of aisle and maneuvering space and shall have a minimum vertical clearance of fifteen (15) feet.
 - a. All loading facilities shall be striped and identified for "loading only."
 - b. The striping and the "loading only" notation or signage shall be maintained in a clear and visible fashion.
3. Pavement Materials. Loading areas shall be constructed with any of the following materials.

Use	Stacking Spaces Required
Bank - Drive Through	4/Window or kiosk
Bank - Drive Through ATM	2/Window or kiosk
Carwash	4/Window or kiosk
Gas Station	2/Pump
Restaurant - Drive Through	4 behind menu & 4 behind first window
All Other	Per Code Administrator

Table 6.F-1. Required Stacking Distances.

- a. Paving materials with a Solar Reflectance Index (SRI) of at least 29.
- b. Pervious pavement.

F. Vehicular Stacking Requirements.

1. General Requirements. Stacking space for vehicles shall be provided according to the requirements in Table 6.F-1 Required Stacking Distances.
2. Location. Stacking spaces for drive-through and other similar Uses should be located in accordance with the Building Type Standards (refer to IV.3).
3. Deviation from Standard. Minimum standards must be adhered unless a traffic impact study (refer to the appendix for details) provides verifiable evidence to allow a reduction. Reductions may be approved by the Code Administrator through site plan review (refer to I.4.H-I).

G. Site Access.

1. General Requirements. These standards shall supplement the provisions for access provided in IV.3 Building Type Standards. Each driveway providing site access from a street, alley, or other vehicular Right-of-Way shall be designed, constructed, and permanently maintained as follows.
 - a. Exception. Driveways providing access to a designated state route shall adhere to the Rules and Regulations for Constructing Driveways on State Highway Rights-of-Way as issued by the State of Tennessee Department of Transportation (TDOT).

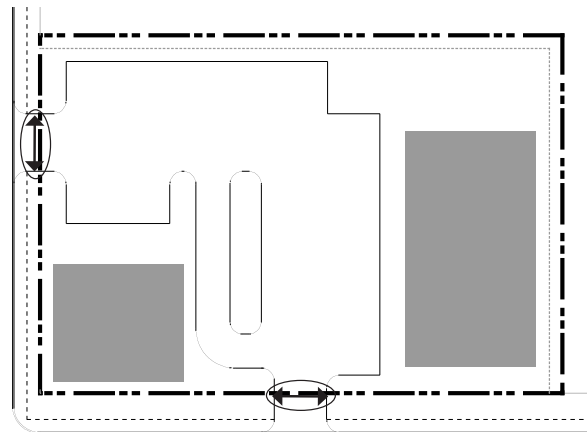


Figure 6.G-1. Driveway Width.

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2. Quantity of Driveways. The number of driveways permitted for each Building Type is located in IV.3 Building Type Standards.
 - a. An additional driveway may be granted through a I.4.H-I Site Plan review.
 - b. When more than one (1) drive is permitted for a Lot, the distance between the driveways, measured along the Right-of-Way, shall not be less than twenty-five (25) feet.
3. Dimensions and Design. Driveway width is measured at the Property Line as is illustrated in Figure 6.G-1 Driveway Width.
 - a. All driveways shall have a maximum width of twenty-two (22) feet except as stated below.
 - (1) Subdivision Type III. Commercial, Mixed Use, and Civic Building Types constructed in Subdivision Type III are permitted to have minimum curb cut widths of fifteen (15) feet.
 - (2) Residential Building Types. Building Types constructed in R3-R8 districts shall have maximum curb cut widths of eleven (11) feet.
 - (3) Industrial Building Types. Building Types constructed in M1-M3 shall have maximum curb cut widths of thirty (30) feet.
 - (4) Maximum width for one-way driveways is eleven (11) feet, except in M1-M3 where a maximum of fifteen (15) feet is permitted.
 - b. Shared Access. When possible, adjacent developments should share points of access to minimize.
 - (1) Shared Driveway Width. When access is shared between three (3) or more non-residential users, a dedicated turn lane may be constructed, allowing an increase in the maximum driveway from twenty-five (25) feet to thirty-two (32) feet provided that:
 - (a) A Traffic Impact Study states its necessity.
 - (b) Access must be onto a Boulevard (refer to II.8.M-N) or Avenue (refer to II.8.K-L).
 - c. Landscape Center. Driveways in residential districts may incorporate a center landscape or Semi-Pervious strip to decrease a site's impervious coverage.
 - d. Drop-Off or Circular Drive. In the R1, R2, and A Districts, a drop-off or circular drive is permitted, provided the following.
 - (1) Circular drive shall not be located closer to the Property Line than the required Setback.
 - (2) No drive shall be closer than fifty (50) feet to the intersection of two streets (the corner).
4. Location. Specific location information can be found in IV.3 Building Type Standards.
 - a. Driveways shall be no closer than two (2) feet from a Side or Rear Property Line, unless the driveway is shared.
 - b. Driveways shall be no closer than three (3) feet from the centerline of a hydrant, light standards, traffic signal, utility pole, or other similar facility that affects traffic operation and regulations.
 - c. Driveway shall not be closer than twenty-five (25) feet from the intersection of two streets (corner), unless otherwise stated in this Section of in IV.3 Building Type Standards.