

**REQUEST FOR QUALIFICATIONS  
U.S. HIGHWAY 70 CORRIDOR PLAN  
CITY OF LAKELAND, TN**

The City of Lakeland seeks qualified consultants with expertise as may be needed in regional, community and land use planning, urban design, architecture, landscape architecture, transportation planning, civil engineering, market/economic analysis, and regulatory methods, for a Corridor Plan for the 3.5 mile stretch of U.S. Highway 70 in Lakeland, TN.

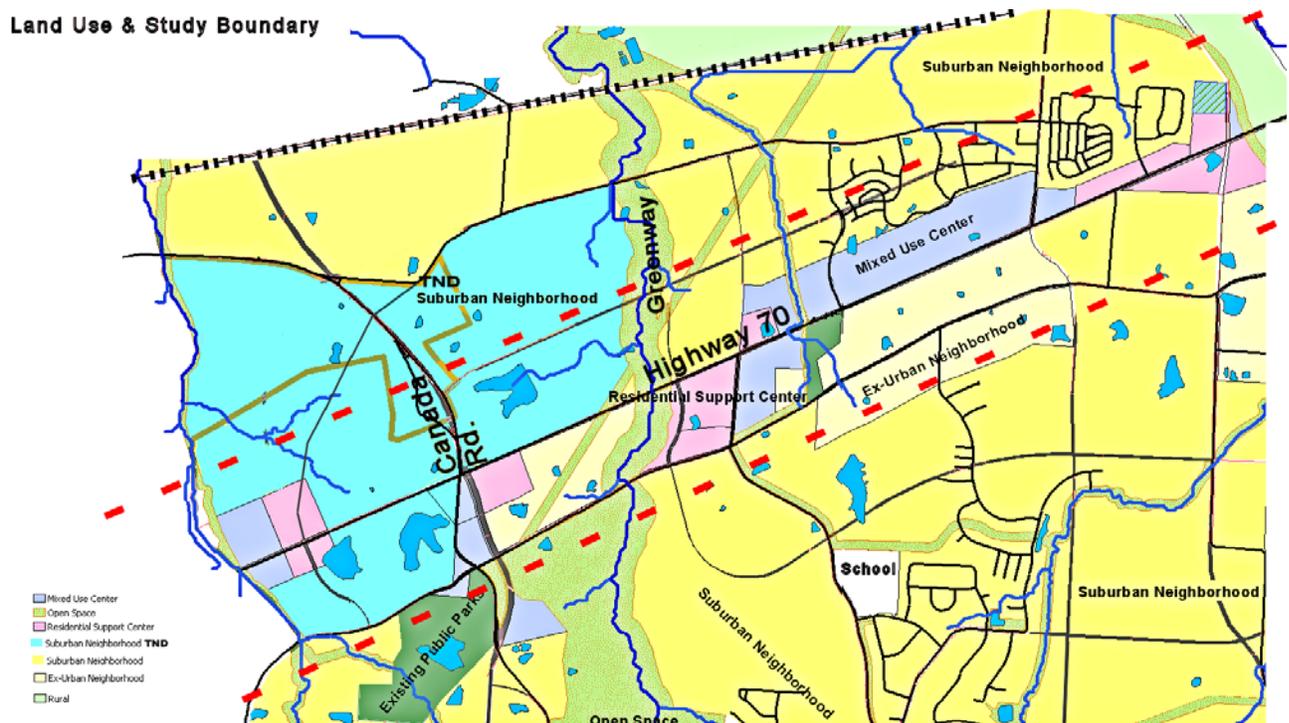
**BACKGROUND**

U.S. Highway 70 is a 4-lane State highway that links a number of suburban communities located in the northeastern part of the Memphis metropolitan area. A 3.5 mile segment of the highway runs through the heart of the City of Lakeland. The preliminary project geography includes areas within 1/4 mile of the highway in Lakeland, a total of over 1,000 acres. A large proportion of this area is undeveloped or lightly developed, but in recent years the area has been under development pressure. When development requests have occurred, it has been evident that Lakeland's citizens have not achieved a common vision for what is acceptable development on the corridor, nor what constitutes a safe roadway. What is clear is that there is a desire that Highway 70 not evolve into a corridor that is similar to busy developed 6+ lane commercial arterials that exist in the region, such as U.S. Highway 64, Germantown Parkway, or Poplar Avenue.

To assist in making future development and transportation decisions, the City of Lakeland desires a plan that establishes a development vision for the corridor, with regulatory and policy actions likely to follow. This plan would differentiate Highway 70 from any other Lakeland corridor by identifying the desired character of development along the roadway as it is built out. Ideally, the plan would include and integrate access management, design controls, roadside, traveled way, and intersection design, circulation, mobility, land use plan refinements, urban form, economic development recommendations, and methods for funding the cost of improvements. If appropriate, multiple typologies along the corridor might be considered, such as for geometry, mobility, or development context. Phasing of the build-out of the roadway should be considered, if appropriate.

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**Figure 1: Highway 70 Current Recommended Land Use and 1/4 Mile Boundary**



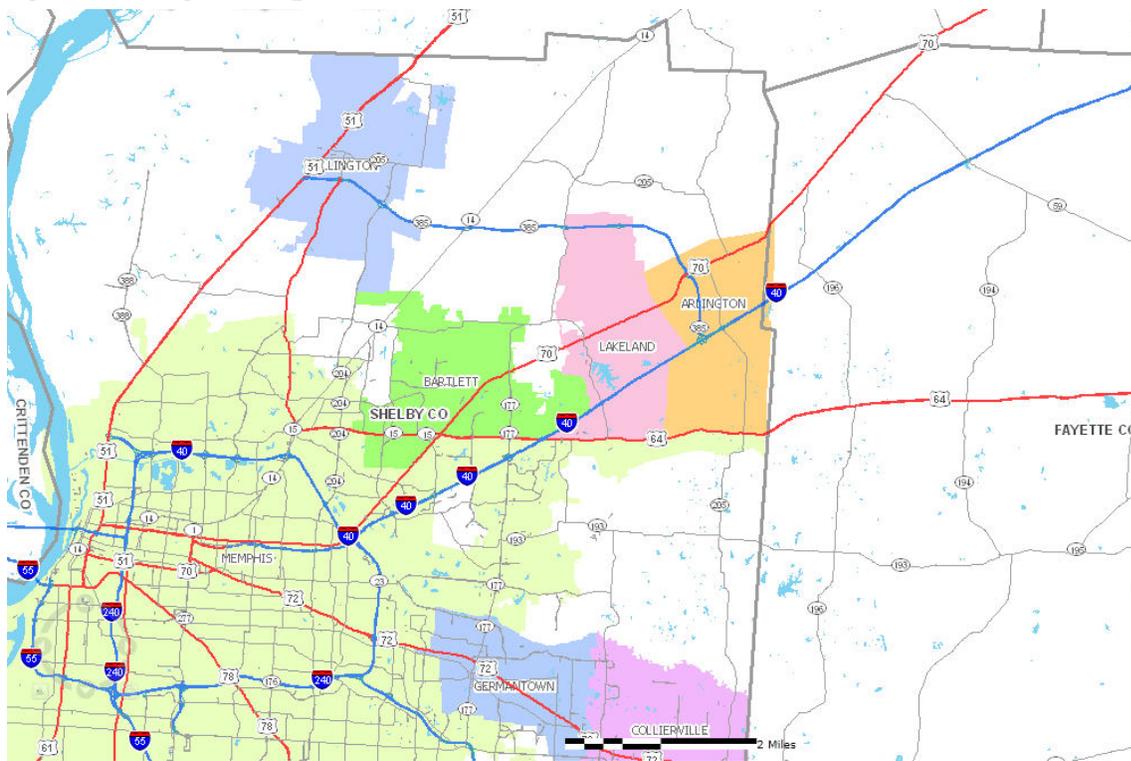
**CITY OF LAKELAND**

The City of Lakeland is a suburban community located within the Memphis metropolitan area, with a 2010 Census population of 12,430. The City is served by Interstate 40 (connecting Memphis to Nashville), U.S. Highway 70, State Highway 385 (I-269), and U.S. Highway 64.

By virtue of its location and planned infrastructure projects locally and regionally, Lakeland could be poised for significant future opportunities, if the community is ready to pursue them. Lakeland's population doubled in the last Census period, earning it the rank of 14<sup>th</sup> fastest growing community in Tennessee. The community comprises over 24 square miles, with an estimated 60% of the land not yet fully developed. The significant amount of available land, high relative household income, high educational attainment, perceptions of a small town with a high quality of life, and location within a growing metropolitan region, suggest future development pressure and opportunity within the community.

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**Figure 2 – Region Map**



Currently, Lakeland has a smattering of retail, service, offices of small professional / technical firms, and an elementary school. Few of the businesses are on Highway 70 to date. Of the businesses on Highway 70, some are considered legal-nonconforming, such as some contractor’s and rental businesses. Most Lakeland businesses are on Highway 64 or at I-40 & Canada Road, several miles to the South. While there is available land in those areas, which often makes them a more immediate focus for commercial development, the area near Highway 70 was a hotbed for residential growth just before the recent economic downturn.

**GUIDING PRINCIPLE**

In addition to principles which may become evident from items examined via the existing conditions report (noted below), or via the public engagement process, the Board of Commissioners of the City have directed that the following guiding principle be utilized.

“The development of the Highway 70 corridor within Lakeland, and areas immediate to it, will create a sense of place which is clearly differentiated from other areas within Lakeland and nearby localities, including that it will be known as a corridor with intrinsic qualities that is rich with amenities, family friendly, one of unique attractions, and that is organized around a theme(s).” .

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**CORRIDOR AND AREA**

In Lakeland, Highway 70 is currently a 4-lane rural section with narrow shoulders. The pavement may be as little as 48 feet wide in some locations (with no bike lanes, sidewalks or roadside paths). Its existing profile may date from the 1920's. Due to somewhat undulating terrain, it has site distance issues in some locations. The future right-of-way for the road has been anticipated by the TDOT to be 108-feet, but the design is indeterminate. Center left turn lanes and signals were introduced at one key Lakeland intersection recently (Canada Rd.), while recent plan approvals for developments not yet constructed have anticipated similar changes for two other intersections within the City limits in coming years. There are no planned or developed pedestrian crossing facilities to date.

The highway connects Lakeland to the adjacent communities of Arlington, TN, and Bartlett, TN. Beyond those cities, it proceeds west into Memphis, TN, and east into relatively rural areas in the next county. Within Lakeland, Canada Road, a significant regional N-S arterial, is slated to be expanded and improved within five years, to the South of Highway 70, connecting Highway 70 to I-40. The interchange at I-40 & Canada Rd. is also slated for total replacement even sooner, beginning construction in 2013. These projects will result in improved connection from Highway 70 to I-40 and then on to Highway 64. In its eastward progression, within two miles of Lakeland's boundary, Highway 70 intersects the future I-269 corridor (currently known as State Highway 385). The "last leg" of the 30-mile Highway 385 is under construction for completion in 2013, providing another linkage to Memphis, and to the most of the major suburban cities of Shelby County.

Lakeland is currently preparing a final draft of new land development regulations. Underlain by the Comprehensive Plan, these regulations would specify land uses, neighborhood types, street frequency (except, generally, for highways), street types, and building types throughout Lakeland, while also providing emphasis on protecting or setting aside natural open space, waterways, and existing mature trees. These may be adopted shortly before this project commences, or while it is underway.

**PROPOSED SCOPE OF SERVICES**

The following represents an outline of services the City is seeking for this project, which could be refined for those asked to submit in the later RFP stage, and with the firm(s) selected when a contract for services is developed.

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**Existing Conditions Report**

The consultant will be expected to prepare an existing conditions report that identifies issues and opportunities to aid with the successful implementation of planning, development standards, road infrastructure improvements, and economic development along Highway 70.

During the data gathering phase, the Consultant will review plans and regulations including, but not necessarily limited to, the following:

1. Existing conditions and existing projects in the Lakeland and Highway 70 vicinity
2. Expected growth in the vicinity including assessing land and transportation development patterns
3. City Comprehensive Plan, including, but not limited to, land use, transportation, recreation, and natural resources elements
4. City zoning map
5. The City economic development strategic plan (in process)
6. City C.I.P.
7. City retail market analysis, 2007
8. City Land Development Regulations (existing, pending)
9. City sign regulations (existing regulations, and new signing needs)
10. City sewer facility locations / types
11. Relevant traffic data and studies on file with the City
12. MPO, Regional Long Range Transportation Plan
13. MPO I-269 Vision Plan (underway)
14. MPO U.S. Highway 70 Plan (announced)
15. Regional Greenprint Vision Plan (underway)
16. Cities of Arlington and Bartlett, comprehensive plans and development policies

The following agencies will be contacted, to determine whether and how they should be included:

1. Lakeland City commissions, elected officials, and staff
2. Lakeland Chamber of Commerce
3. Greater Memphis Chamber of Commerce
4. MLGW (regional utility), providing electric, gas, and water
5. Tennessee Dept. of Transportation
6. Adjacent cities and their offices (e.g., planning, sustainability, engineering)
7. MPO

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**Corridor Planning**

The core of this project is to integrate land use, transportation planning, and economic development recommendations in an agreed vision for preferred development, including:

- Prepare a strategic plan that outlines goals and objectives for development along the corridor, integrating the Comprehensive Plan, market analysis, corridor design, access management, results from the public engagement process, and Consultant's expertise;
- Identify preferred development patterns along the corridor in terms of use, compatibility with existing and planned development, scale, design and transportation impacts;
- Make recommendations for new or revised development codes based on approved development;
- Make recommendations for transportation improvements based on anticipated traffic growth from new development;
- Provide an action plan to implement strategies, and that allows us to be prepared as growth continues. Implementation actions should include a proposed schedule for making improvements;
- Provide funding alternatives, such as, but not limited to, impact fees, adequate facilities taxes, TIF, City, and private funds.

**Corridor Design**

The Consultant will develop corridor design guidelines consistent with the corridor strategic plan, goals, and objectives. The Consultant will illustrate street designs that incorporate, complement, introduce, or refine environmental, natural resource, safety, roadway, multi-modal mobility (including potential highway crossings), urban form, and aesthetic aspects (e.g, roadside amenities, views and other intrinsic qualities, lighting, landscaping) to the highway area. The Consultant will provide preferred typologies for areas along the roadway and environs that create a unique identity for the corridor. The City may consider using these at a later time to update regulations.

**Access Management**

The Consultant will develop access management requirements for the corridor, based upon projected land use patterns and roadway functional classifications. Requirements considered shall include, but are not limited to, driveway spacing, signalized intersection spacing, left turn lanes, acceleration lanes, deceleration/right turn lanes, driveway widths, radii, and profile; safe stopping sight distances, and raised channelization islands.

The Consultant will review the City's codes and procedures when traffic impact studies are warranted and make recommendations for improvements, including how to incorporate access management into studies, if necessary.

Work integrally with TDOT to determine whether desired changes can be implemented.

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If beneficial and appropriate, the Consultant will identify ways to relax regulatory requirements (zoning, subdivision, or other) if developers agree to incorporate access management practices. Ordinance coding and processes to adopt new codes, if any, would be done by City staff at a later time.

The recommendations should contain an implementation plan that establishes priorities, responsible parties (e.g., City, TDOT, or future land developers), and potential funding methods for completing selected improvements.

**Market and Fiscal Analysis**

The Consultant will examine the correlation between the market, competitive factors, and revenue potential based on land use scenarios for key sites. The Consultant will indicate implementation actions to pursue economic development goals that are unique to the corridor, and consistent with the corridor strategic plan, goals, and objectives.

**Public Participation**

The Consultant will conduct public engagement to solicit input and guidance on preferred development patterns along the corridor. The organizational methodology for the plan should include public participation elements and meeting schedules.

**Technical Reports**

The Consultant will work with staff while a proposal is being developed to identify which of the above steps require a technical report submission during the project.

**RFQ SUBMISSION**

All inquiries should be made to the Planning Department. 10 copies of responses shall be provided, and shall not be returned or reimbursed for cost by the City of Lakeland. With an exception allowed for attachments, please provide an additional copy of the RFQ response on CD / DVD. Project samples provided for review shall be returned by the City, only if requested, once a candidate firm is selected and approved to conduct the project.

Limit responses as closely as possible to 30 pages or less, and include at a minimum, the following. If provided, full work samples shall be separate, may exceed the 30-page maximum, and shall be attached separately to the end of the submission. Provide the response in the order given below.

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- ✓ Cover letter identifying lead and sub-consultants and roles, and their ability to complete work in the timeframes indicated.
- ✓ Describe your firm(s), firm history(s), and team organization and strengths.
- ✓ Breakdown of key personnel and their individual qualifications and roles. Also provide resumes.
- ✓ Provide your interpretation of the scope, and a project approach that can be expected to be given in more detail if you are subsequently asked to provide an RFP response.
- ✓ Provide descriptions of corridor plans or very similar projects conducted within the last 5 years, and how they demonstrate your expertise and the breadth of your capabilities for this type of project. Identify which consultant or sub-consultant personnel worked on the projects, and their roles.
- ✓ Provide references, especially for similar integrated projects.
- ✓ Provide work samples as separate attachments
- ✓ List a single person to contact for information. Include mail, email, phone, and fax.

**Inquiries, RFQ Evaluation, and Schedule**

An RFQ Committee will be formed to evaluate responses. The City staff responsible for guiding an RFQ Committee may or may not include the Growth Management Director, City Engineer, Natural Resources Director, Planning Director, and City Manager. Others may be added to the committee before inception.

RFQ responses will be evaluated and ranked, and selected candidates may be invited to interview by phone or teleconference. Respondents that are interviewed by the Committee may be offered an invitation to provide a proposal.

The City reserves the right to accept or reject any response prior the RFQ, and the subsequent RFP.

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**Anticipated schedule (approximate)**

**Request for Qualifications**

1. RFQ solicitation, October 1, 2012
2. RFQ response due, October 19, 2012
3. Notice to shortlisted firm(s), November 2, 2012
4. Phone conferences with shortlisted firms on or before November 16, 2012

**Request for Proposals**

5. Notify selected consultant firm(s) to request RFP responses, and provide RFP requirements to them, November 19, 2012
6. RFP response due, December 13, 2012
7. Interview(s) with selected respondents, dates to be determined
8. Preliminarily select consultant firm(s), January 7, 2013

**Begin project**

9. Develop detailed scope, fee, and contract for services, January 21, 2013
10. BOC approval of selected firm(s) & contract for services, February 7, 2013
11. Commence work, TBD, 2013
12. Project completion, TBD, 2013

Contact information

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Additional information may be provided upon request, or may be found on the City's website - [www.lakelandtn.gov](http://www.lakelandtn.gov)